

For the information of Railway Staff only.



EASTERN REGION

**SUPPLEMENTARY
NOTICE
of
SIGNALLING
ALTERATIONS**

affecting the working of the line

from

SUNDAY 4 NOVEMBER, 1984.

**CHESTERTON JUNCTION
and WATERBEACH (exclusive).
- also,
the Fen Drayton Branch.**

York

MO42-7001

R.M. Williams

Regional Operations Manager

November 1984

SIGNALLING RECORD SOCIETY

www.s-r-s.org.uk

DIGITAL ARCHIVE

This PDF Copy has been provided free of charge by David Allen in order to assist your research into UK signalling.

This file is one of a number scanned by David Allen using material from his own collection and from the collections of Phil Deaves, Robert Dey, David Ingham, Simon Lowe, John McCrickard, John Midcalf, Roger Newman, Richard Pulleyn and Chris Wolstenholmes. Thank you one and all. Many of the original documents are now in the SRS Archive or at the National Railway Museum.

You may also like to provide copies of Signalling Notices and Weekly (and periodical) Operating Notices as scans or as originals. The SRS is always willing to accept donations of any signalling or signalling related material for inclusion in the Society's Archive. Please contact the [Archivist](#) in the first instance.

For a list of PDFs currently available visit the list of [Weekly](#) Notices or [Signalling](#) Notices page.

If you have benefited from this PDF copy, why not [join](#) the Signalling Record Society and receive support for your researches and access to the Society's Archives

Members receive "The Signalling Record" six times annually plus a newsletter and have the opportunity to purchase SRS [books](#) and other [publications](#) at a discount. They also have access to back issues of The Newsletter and The Signalling Record which are only available to members. These contain a wealth of information accumulated since 1970, much of which is not readily available anywhere else.

In addition, Members have the opportunity to join signalling related visits to locations on the UK national and London Underground systems; and to other UK Railways.

To join the Signalling Record Society visit

www.s-r-s.org.uk/membform.html

CHESTERTON JUNCTION - WATERBEACH (EXCLUSIVE) RESIGNALLING, INCLUDING THE FEN
DRAYTON BRANCH

On Sunday 4th November - Chesterton Junction Signal Box will be abolished.

New colourlight signalling will be commissioned. The Track Circuit Block Regulations will apply with Waterbeach as the outpost to CAMBRIDGE Signal Box.

Chesterton Junction (and all other points previously controlled by Chesterton Junction Signal Box) will be brought under control of Cambridge Signal Box.

Alterations to Existing Signalling

Replating of Signals

<u>Line</u>	<u>Old (Chesterton Junction) Number</u>	<u>New (Cambridge) Number</u>
Down Main	CJ52	CA211 (see below)
Up	CJ15	CA212

The Waterbeach Down Main 2-aspect Colourlight Distant, will be replaced by a 3-aspect Colourlight Automatic signal plated CA231 and repositioned 180 yards nearer to Waterbeach Signal Box. This signal will also act as the Waterbeach Down Distant, 1,395 yards from the Waterbeach Down Home Signal.

The Waterbeach Semaphore Up Starting signal will be abolished.

Description of Routing Signals CA = Cambridge

<u>Signal No.</u>	<u>Line</u>	<u>Aspect</u> M = Main PL = Position Light	<u>Route Indication</u>	<u>Application to or towards</u>
<u>Down Direction</u>				
CA211	Down Main	M PL	"B"	CA225 Fen Drayton Branch
		PL	"E"	Engineers Depot
		PL	"D"	Central Depot
CA223	Engineers Depot Reception Sidings	M PL		Down Main CA229 Shunt Spur

New Down Main Automatic Signal CA231 will also function as the Waterbeach Down Distant.

Up Direction

CA228	Up Main	M PL		CA220 Engineers Depot Reception Sidings.
CA214	Up St. Ives	M		Up Main CA212 (former CJ15)

A reflectorised Distant board will be provided 390 yards before reaching CA214.

/Cont'd

Cont.....

Automatic Warning System

A.W.S. will be provided on all running signals between Coldham Lane Junction inclusive and Waterbeach (exclusive). The Inductors associated with CA214 signal and its associated Distant board will not be suppressed when passed over in the Down direction.

A.W.S. cancelling indicators will be provided applicable to Down trains, 200 yards after passing these inductors *on the Fen Drayton Branch line.*

New Position Light Signals Controlled from Cambridge

<u>Signal No.</u>	<u>Line</u>	<u>Route Indication</u>	<u>Destination</u>
751	Up Main	"B" "E" "D" "M"	Fen Drayton Branch Engineers Depot Central Depot Down Main
752	Down Main		Up Main
754	Central Depot	"M" "X"	Up Main Down Main "LIMIT OF SHUNT" indicator
756	Engineers Depot	"M" "X"	Up Main Down Main "LIMIT OF SHUNT" indicator
758	Shunt Spur		Engineers Depot Reception Sidings.

A "LIMIT OF SHUNT" Indicator will be provided on the Down Main line 280 yards South of CA211 signal.

"STOP TELEPHONE" notice boards will be provided to control Down direction movements within the Engineer's Depot Reception Sidings.

Taylor/Shellmex ground frame (previously released from Chesterton Junction Signal Box) will now be released from Cambridge. Telephone communication will be provided between the ground frame and Cambridge signal box).

Waterbeach

The trailing crossover will be secured out of use in the normal position except when required for use in emergency.

/Cont'd

Cont.....

Level Crossings

Chesterton Junction Level Crossing Across the Main Lines at 57m. 54chs.

The level crossing lifting barriers will be supervised from Cambridge Signal Box with the aid of Closed Circuit Television.

Milton Fen Level Crossing Across the Main Lines at 59m. 10chs.

Milton Fen Gate Box will be abolished and the level crossing will be converted to an Automatic Open (A.O.C.R.) Crossing remotely controlled from Cambridge. Telephone communication will be provided with Cambridge signal box..

A "WHISTLE" board will be provided 257 yards from the crossing on each rail approach.

FEN DRAYTON BRANCH

Between Chesterton Junction and the Branch Staff release instrument 388 yards before reaching Milton Road Level Crossing at 58m. 38chs. the single line will be operated in accordance with the Track Circuit Block regulations. The remainder of the Branch will be operated in accordance with the "ONE TRAIN WORKING REGULATIONS".

A new staff instrument, released from Cambridge Signal Box will be provided at approximately 58 $\frac{1}{4}$ m.p. (Telephone communication will be provided with Cambridge signal box).

A double-sided notice board immediately beyond the Staff Instrument will be provided. The wording of the side of this notice board applicable to trains approaching in the Down direction will be :-

"START OF SINGLE LINE. COLLECT STAFF FROM INSTRUMENT BEFORE PROCEEDING".

The opposite side (applicable to Up trains) will be worded :-

"END OF SINGLE LINE. REPLACE STAFF IN INSTRUMENT".

Milton Road (Trainmen Operated) Level Crossing at 58m. 38chs. (Across the Down/Up Fen Drayton Branch Single Line)

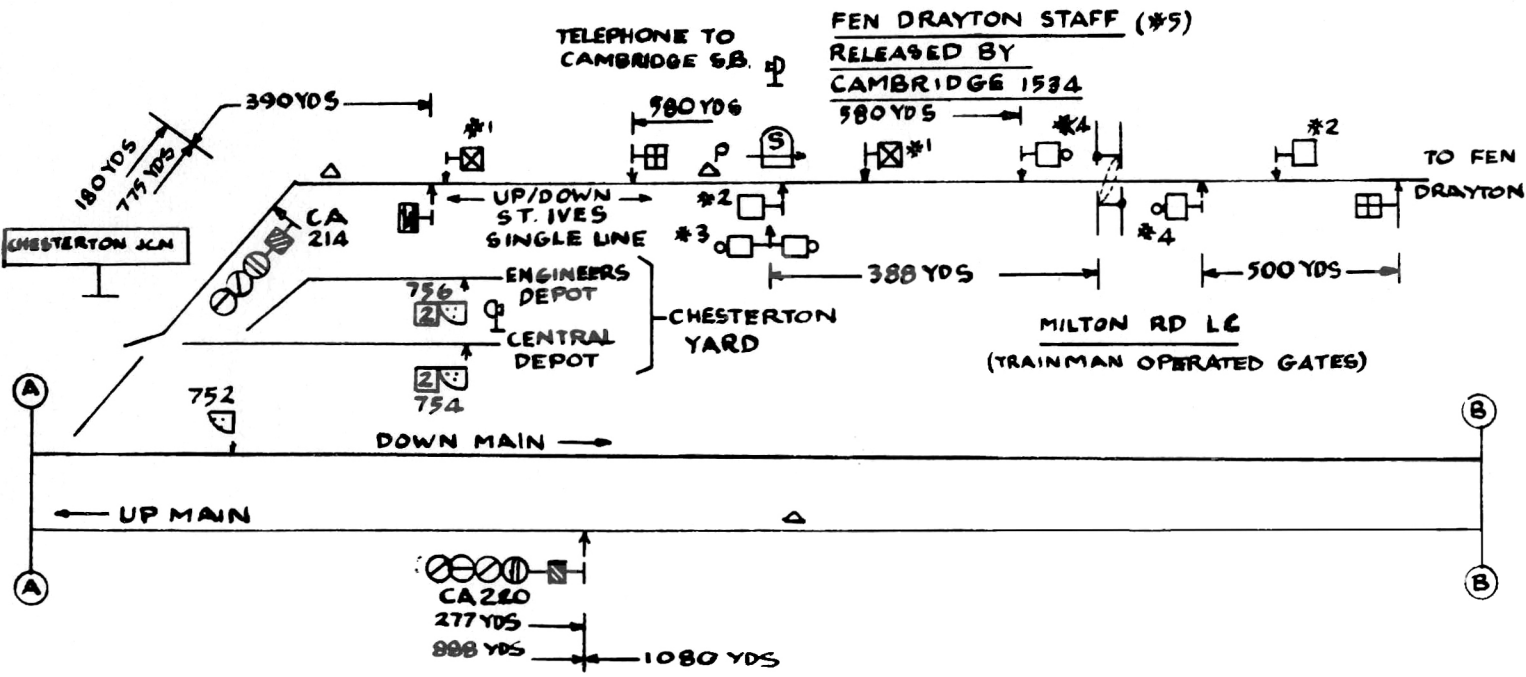
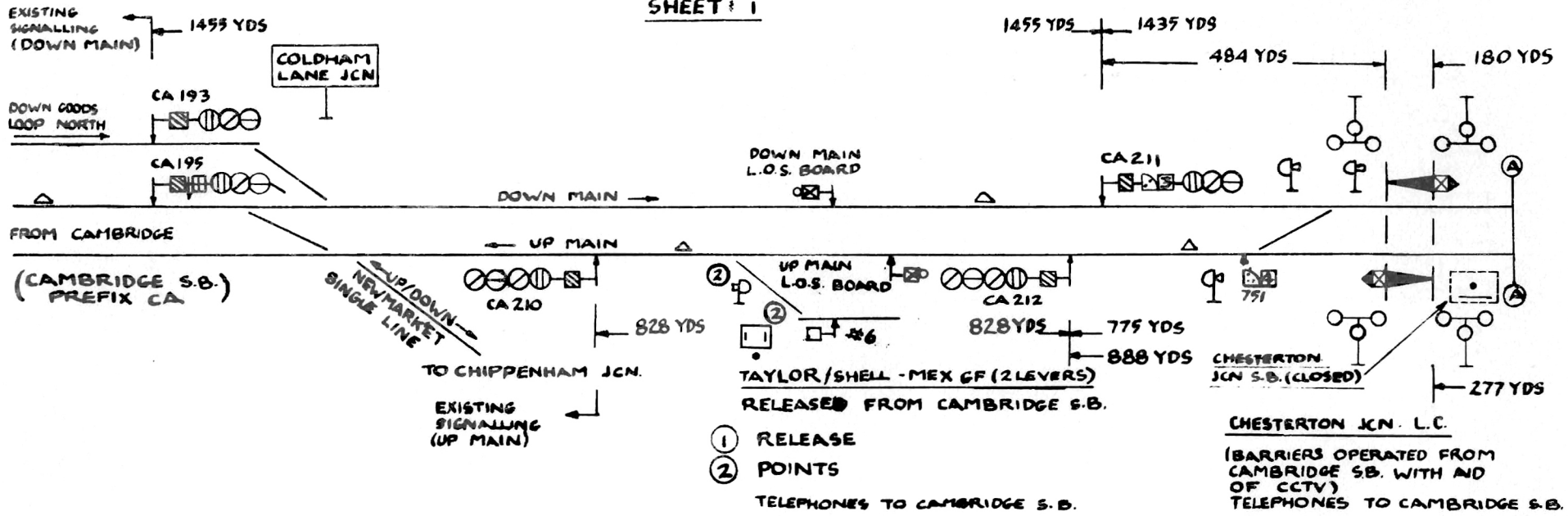
A notice board will be provided adjacent to the level crossing on each rail approach worded:-

"OPEN CROSSING GATES BEFORE PROCEEDING"

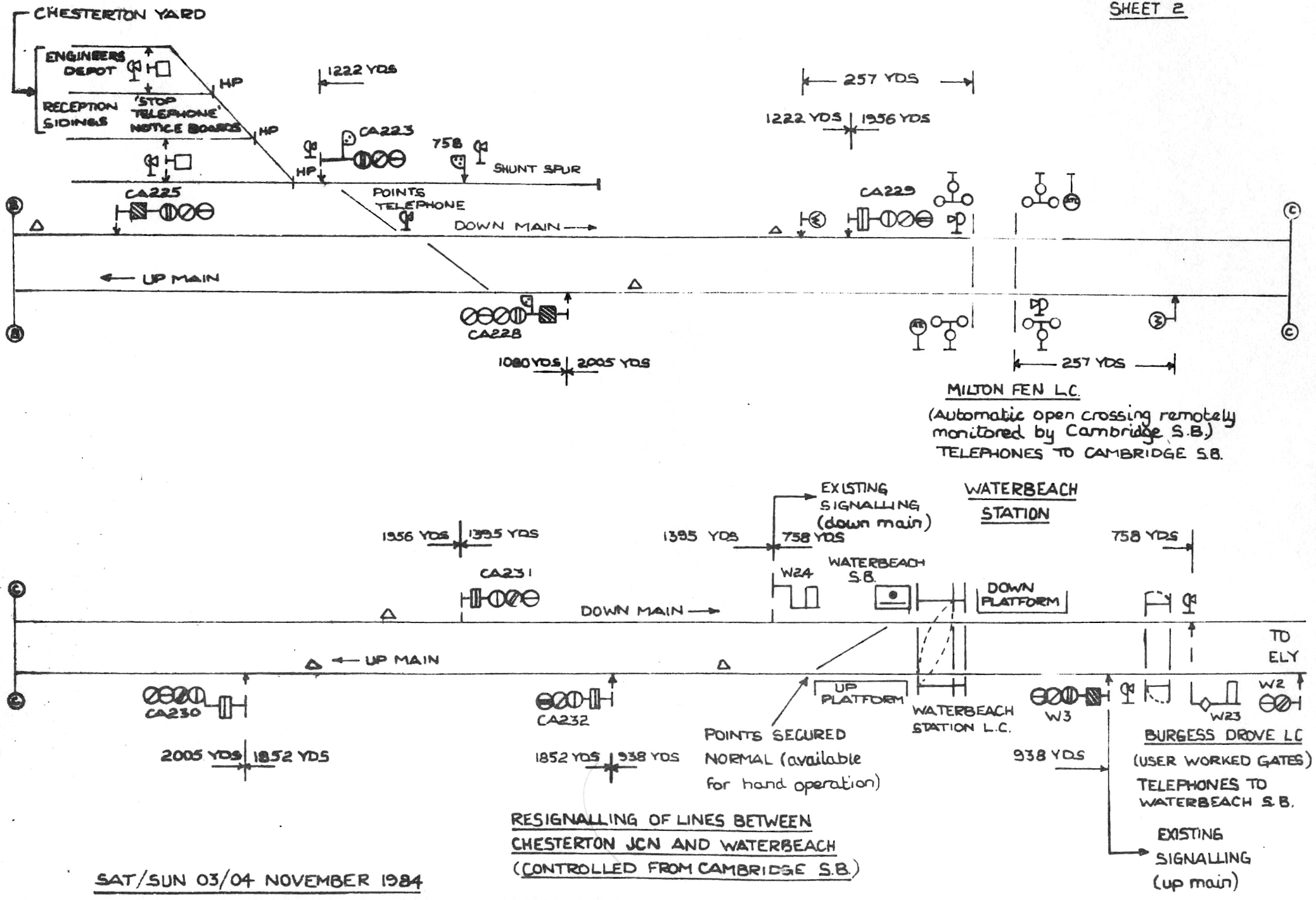
A black St. George's cross on a white background will be provided 500 yards before reaching this notice board on the Up approach and 580 yards before reaching the notice board on the Down approach.

A "52" wagon marker board will be provided 388 yards after passing Milton Road Level Crossing in each direction.

The above should be read in conjunction with the Weekly Operating Notice and the Diagrams attached hereto.



- *1 AWS CANCEL INDICATION BOARD
- *2 52 WAGON MARKER BOARD
- *3 BACK TO BACK STOP BOARDS WITH SUPPLEMENTARY NOTICES UP DIRECTION 'END OF SINGLE LINE REPLACE STAFF IN INSTRUMENT' DOWN DIRECTION 'START OF SINGLE LINE COLLECT STAFF FROM INSTRUMENT BEFORE PROCEEDING'
- *4 STOP BOARD WITH SUPPLEMENTARY NOTICE 'OPEN CROSSING GATES BEFORE PROCEEDING'
- *5 STAFF FOR AUTHORITY TO ENTER SINGLE LINE IS IN ANNETTS KEY FORM NORMALLY KEPT LOCKED IN KEY RELEASE INSTRUMENT.
- *6 NOTICE BOARD WORDED:- 'DRIVERS NOT TO PASS THIS BOARD WITHOUT AUTHORITY'



SAT/SUN 03/04 NOVEMBER 1984